



# BUairmail

# Manual

# 2026





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This training manual is intended as support for employees of the Postal Authorities who build up ULDs with airmail.

This training manual is intended to be used as a self-study, and contains the Lufthansa Cargo rules for safety.

We ask you to read through the chapters and become aware of the rules and regulation for the build-up of airmail BUP ULDs.





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# 1. Ground Operations Manual

## Introduction

Every day hundreds of Lufthansa aircraft take off and land. They carry thousands of passengers, tons of mail and even more tons of cargo to almost all corners of the world.

Our **customers** rely on Lufthansa Cargo to transport their cargo and mail without undue delay or damage.

Our **passengers** rely on Lufthansa to transport them and their baggage in order that they reach their destination safely.

Our **crews** rely on the correct and safe loading of the cargo in our aircraft.

All of them expect that whatever has been loaded inside the aircraft has been done so according to the regulations.

With this manual, you will get to know the safety factors involved when building up ULD, as well as **YOUR responsibility** towards your/our customers, our passengers, crew and aircraft.

These guidelines are based on the LCAG rules from the AHM (Aircraft Handling Manual) and CHM (Cargo Handling Manual).





## 2. General Requirements

To ensure the required flight safety and security the Postal Authority declares that he is aware of the following additional requirements to the Training Manual:

1. The pre-built units shall not contain any of the following items:
  - All weapons, weapons parts and ammunition
  - Cargo subject to special security measures, e.g. valuable cargo (VAL), as example gold and precious stones, as well as vulnerable goods (VUN), as example cellular phones and silver
  - Perishable goods (PER)
  - Live Animals (AVI)
  - The ICAO TI and the UPU Convention prohibits the transport of dangerous goods in airmail. There are a few dangerous goods exceptionally admitted in airmail (see ATA-DGR 2.4) under condition that these are in compliance with specific provisions in the ICAO TI and UPU manuals. It is in the responsibility of the national Postal Administration to ensure that the regulations and provisions for exceptionally permitted dangerous goods are complied with.
  - Airmail exceeding the base dimension of the ULD.
  - Metal sheets and metal plates; plates made out of other materials with individual weight of more than 20 kg
  - Metal pipes and metal bars; pipes and bars made out of other material are allowed as per loading requirements stated in the Training Manual
2. All packages containing Lithium ion, sodium ion or lithium metal cells and batteries meeting the provisions of Section II of Packing Instructions 966, 967, 969, 970, 977, 978 must be loaded in units with lower deck contour.
3. Only transparent plastic foil shall be used for pallets to allow visual inspection upon acceptance by LCAG.
4. It is not permitted to use air or gas filled dunnage bags to secure the load inside a ULD.





5. Build-up must be done according to the specific requirement in this Manual.
6. On the maindeck of freighters airmail may only be built-up
  - B777F and A321F: in fire-resistant containers (FRC)
  - B777F only: on pallets with a fire-containment cover (FCC)
7. This BUP program only allows the loading of single mailbags that - with regards to weight and dimensions - must comply with UPU rules.
8. Prerequisite: CARDIT  
It is mandatory that LCAG receives a CARDIT message in the latest version. The CARDIT must be sent before the handover of the airmail to LCAG and must contain
  - the nesting information,
  - the relevant security information,
  - the correct flight information,
  - AR flag (non EU origin stations).





## Qualification Requirements

Staff members responsible for the **ULD build-up** must be qualified as below:

1. Valid DG license IATA 7.4 or CBTA equivalent.

Refresher training requirements as per IATA DGR 1.5 CBTA must be considered and followed.

2. A sufficient number of employees, who control and supervise the ULD build-up and sign the ULD-tag, must hold a valid BUPairmail Qualification ("BUP Airmail" WBT available online).

A refresher training must be done latest every 24 months.

The BUP qualifications are **always** applicable to the participant, not the Postal Authority.

Required documents and additional information can be obtained from the local Lufthansa Cargo AG office.

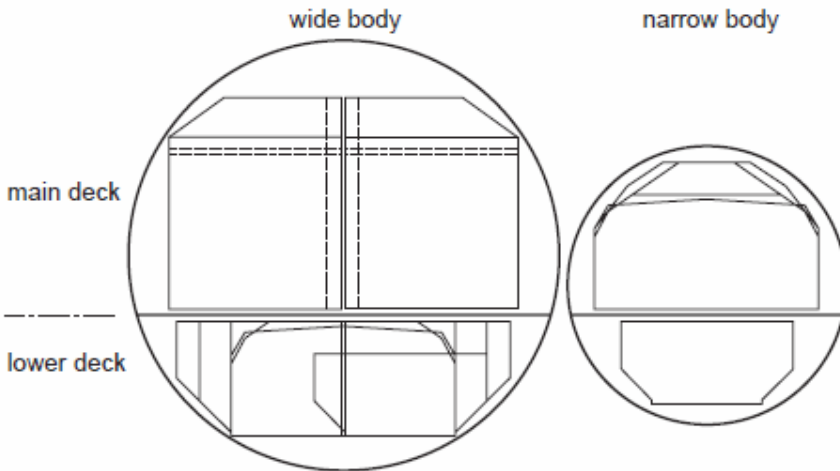




### 3. Aircraft Terminology

Aircraft can be divided up into several categories:

- wide body and narrow body
- passenger aircraft (carrying passengers and cargo) and freighter aircraft (carrying only cargo)



Examples of narrow body aircraft, that hold ULDs, are the A320, A321 and A321F.

Examples of wide body aircraft are: A330, A340, A350, B747, B787 and B777F

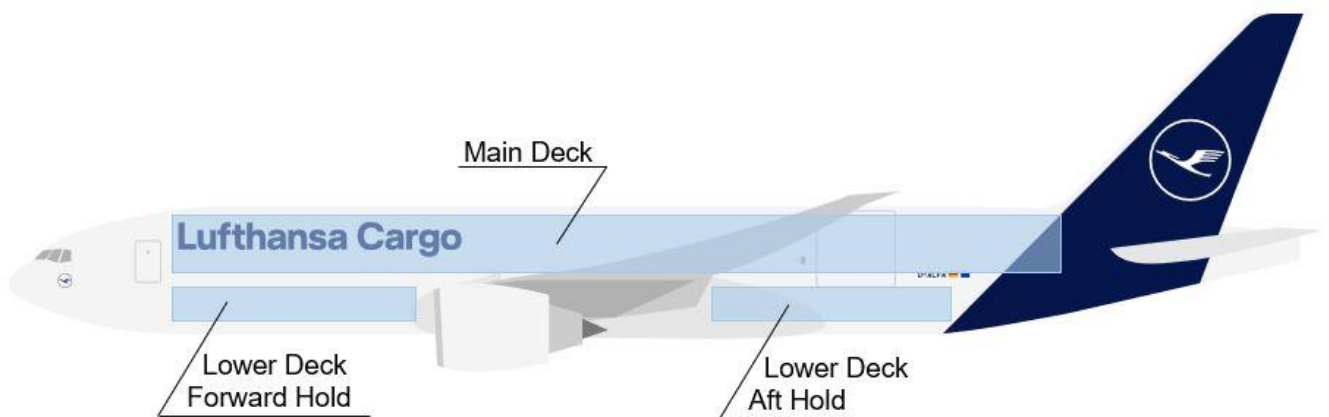
Every aircraft is divided into 2 sections: Main Deck and Lower Deck

In a passenger aircraft, the passenger cabin is located on the Main Deck. For the freighter aircraft, the Main Deck is a cargo hold.

The Lower Deck on both the passenger and freighter aircraft is broken into 2 holds: Forward and Aft hold.

The Forward and Aft hold are broken down further into compartments, which may be numerically different depending on the aircraft type.

Here is an example of a freighter aircraft:





## 4. ULDs, Fleet, and Contours

ULDs and Contour for narrow body aircraft:

### A320/A321/A321F

Containers

Code: **AKH / AKW**



Base dimension: 153 x 156cm / 60,4 x 61,5in

Height: 114cm/45in

Max Gross Weight 1000kg





Pallet

Code: **PKC**



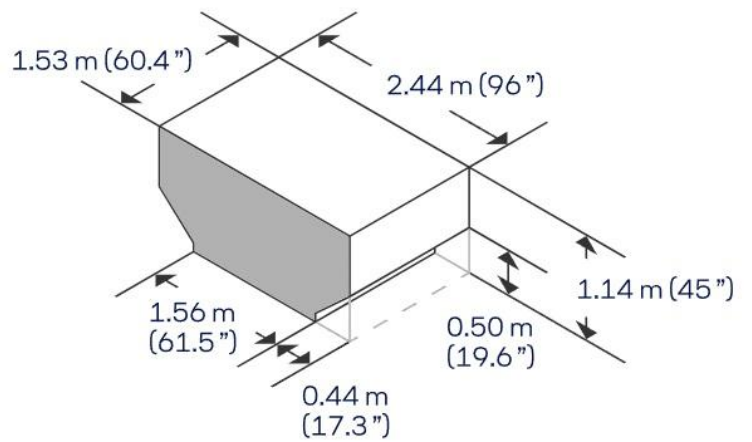
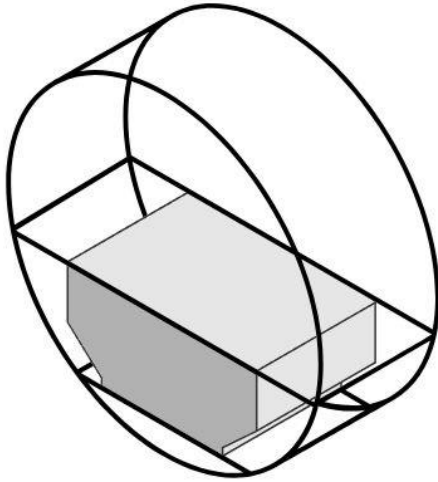
Base dimension:	153 x 156cm / 60,4 x 61,5in
Height:	114cm/45in (including base of pallet)
Max Gross Weight	1000kg

For all ULD's with permanent fixed nets: please do not try to remove, cut or exchange the nets.  
A damaged permfix-net equals a damaged pallet. Consequently, mark the ULD with a damage tag and file a damage report.  
Please make sure to keep distance between the pallets when building a pallet stack by using wood or bars.



## Maximum Contour A320/A321/A321F Lower Deck

The H contour is the maximum contour on the lower deck of all A320/A321/A321F.





## Narrow Body Freighter A321F Main Deck

Container

Code: **AAY**  
Base dimension: 224 x 318cm  
88 x 125in  
Height: 208cm/88in  
Max. Gross weight: 1800kg



**Loadable** only in Main Deck A321F





## ULDs for wide body Aircraft:

Container

Code: **AKE (LD3)**



Base Dimension: 153 x 156cm / 60,4 x 61,5in

Height: 163cm/64in

Max Gross Weight: 1500kg





Container

Code: **AMP**



Base Dimension: 244 x 318cm / 96 x 125in

Height: 163cm/64in

Max Gross Weight: 5000kg





Container

Code: **AMJ**



Base dimension: 244 x 318cm / 96 x 125in

Height: 244 cm/96in

Max. Gross weight: 5800kg

Only the fire resistant AMJ containers (AMJ 49000 LH - AMJ 49999 LH) may be used for the build-up of airmail.

Loadable only in Main Deck of the B777F





Pallet

Code: **PAJ**



Base dimension: 224 x 318cm / 88 x 125in

Height	Contour	Maximum Gross Weight	Aircraft
163cm/64in	P	4000kg	Lower Deck wide body Aircraft
300cm/118in	J4	5000kg	Main Deck B777F only with a fire-containment cover (FCC)



Pallet

Code: **PMC**



Base dimension: 244 x 318cm / 96 x 125in

Height	Contour	Maximum Gross Weight	Aircraft
163cm/64in	P	5000kg	Lower Deck wide body Aircraft
300cm/118in	J4	5800kg	Main Deck B777F only with a fire-containment cover (FCC)





Pallet

Code: **PLW/PLA**



Base dimension : 153 x 318cm / 60,4 x 125in

Contour: P or F

Capacity per extension: 350kg (only PLW)

Max Gross Weight: 3000kg

Loadable in the Lower Deck of all wide body aircraft, depending on the contour built (except Freighters)

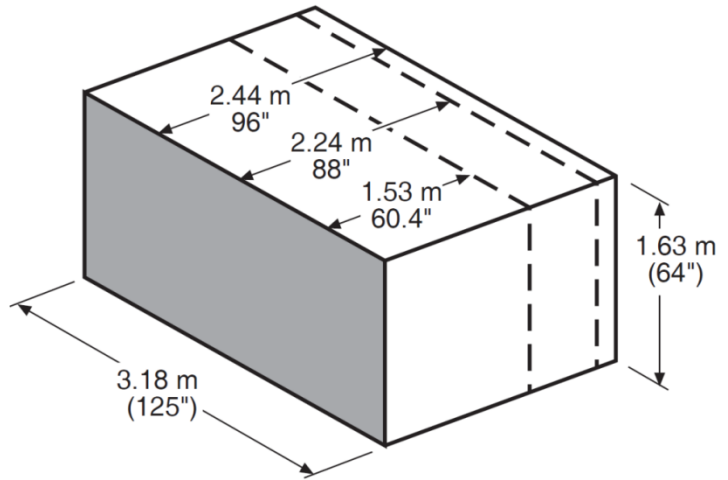
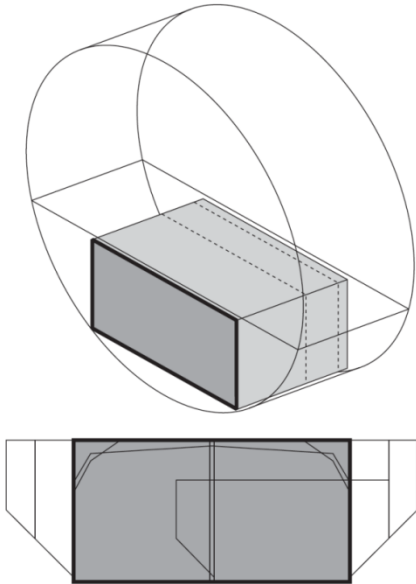
PLA: pallet without side extensions





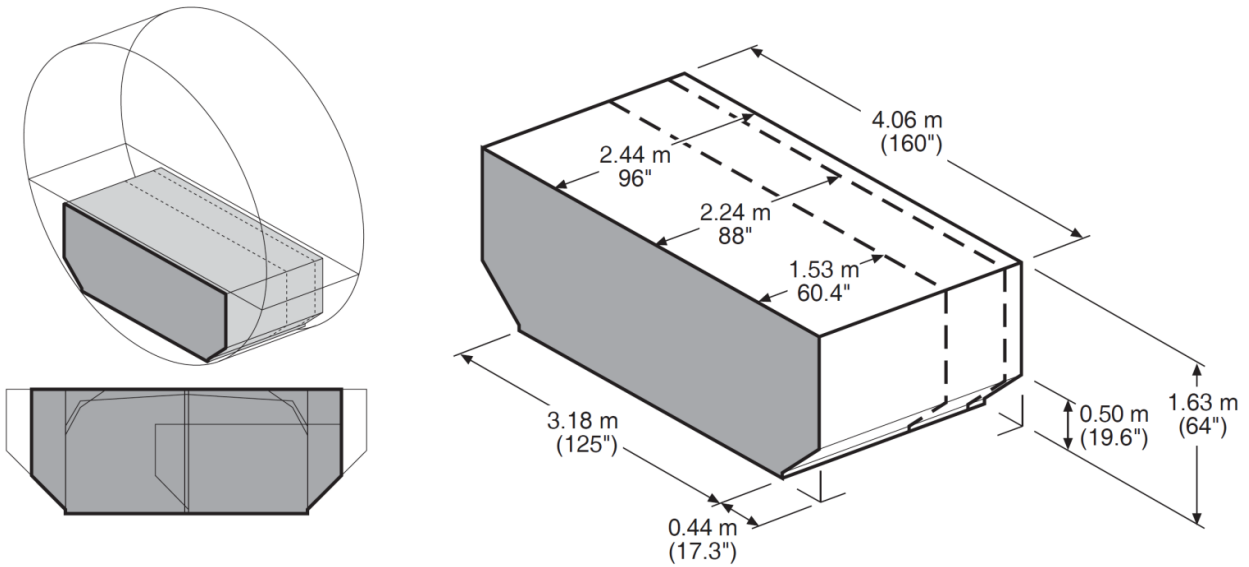
## Lower Deck Contour wide body aircraft

### P contour



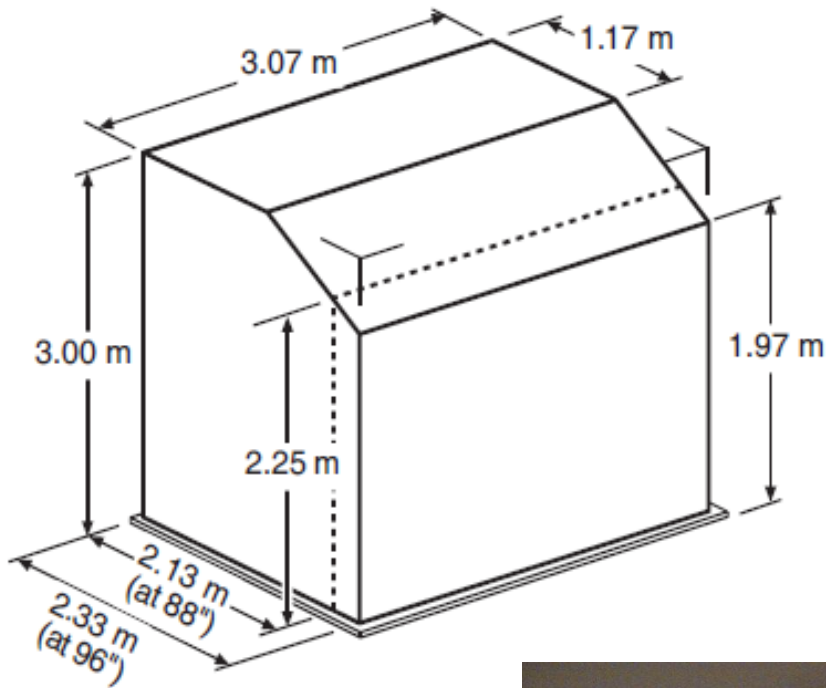


**F contour**





**Maximum Main Deck Contours Freighter aircraft B777F**



**J4**  
**(B777F)**





## ULDs and Fleet Summary Sheet

### Lufthansa Cargo ULDS

#### Pallets

Code	Base Dimensions	Net Type	Notes
PAJ	224 x 318cm 88 x 125"	NME	Lower Deck or Main Deck (with fire-containment cover, FCC) wide body aircraft
PMC	244 x 318cm 96 x 125"	NME	Lower Deck or Main Deck (with fire-containment cover, FCC) wide body aircraft
PLW/PLA	153 x 318cm 60.4 x 125"	NLA	350kg maximum per side extension Lower Deck all wide body passenger aircraft
PKC	153 x 156cm 60,4 x 61,5	NKA	Lower Deck A320/A321 only

#### Containers

Code	Base Dimensions	Height	Notes
AKE	153 x 156cm 60,4 x 61,5"	163cm 64"	Lower Deck wide body aircraft Also called a LD3
AKW/AKH	153 x 156cm 60,4 x 61,5"	114cm 45"	Lower Deck A320/A321 only
AMP	244 x 318cm 96 x 125"	163cm 64"	Lower Deck/Main Deck all wide body aircrafts
AMJ	244 x 318cm 96 x 125"	244cm 96"	Main Deck for freighter only (only fire resistant containers (AMJ 49000 LH - AMJ 49999 LH))
AAY	224 x 318cm 88 x 125"	163cm 82"	Maindeck A321F only (all AAY containers are fire resistant)

## ULDs, Fleet & Contours Summary Sheet





Passenger	Loadable ULDs & Maximum Gross Weight				
	PKC	AKH, AKW	Maximum Contour	Maximum Height	
A320	1000	1000	H	114cm	
A321	1000	1000	H	114cm	

Passenger	Loadable ULDs & Maximum Gross Weight					
	88"	96"	LD3	PLW/PLA	Possible Contour	Maximum Height
A330-300	4000	5000	1500	3000	P or F	163cm
A340-300	4000	5000	1500	3000	P or F	163cm
A350-900	4000	5000	1500	3000	P or F	163cm
B747-8	4000	5000	1500	3000	P or F	163cm
B787-9	4000	5000	1500	3000	P or F	163cm

Freighter	Loadable ULDs & Maximum Gross Weights					
Lower Deck	88"	96"	LD3	60,4x61,5" A321F	Maximum Contour	Maximum Height
A321F	--	--	--	1000	H	114cm
B777F	4000	5000	1500	--	P	163cm

Freighter	Loadable ULDs & Maximum Gross Weights				
Main Deck	88"	96"		Maximum Contour	Maximum Height
A321F	1800	--	--	Y	208cm
B777F	5000	5800		J4	300cm

All ULDs/Contours, which are not mentioned in Chapter 4, may not be used.



## 5. Serviceability, Storage and Transportation

LCAG checks ULDs prior to their release to an Agent/Customer. Nevertheless, you must double check any ULD for their serviceability before loading.

Any damages to ULDs, making it unserviceable, can potentially affect the security and safety of the cargo loaded within the ULD, as well as the aircraft on which it is loaded on.





These pictures show a selection of repair events that happen every year and the approximate cost for such repairs.





All regulations of this subsection are also valid for ULDs and tie-down material provided by other airlines when transported in Lufthansa or Lufthansa Cargo aircrafts, if there are no deviating regulations in specific paragraphs

## Basic Rules of all ULDs

**Certification:** Type label or type markings must be fixed or engraved **and** readable as proof of certification. All ULDs must be certified by either ETSO (JTSO) or TSO.

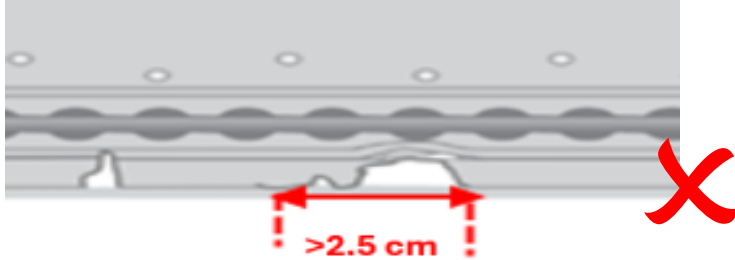


(pallets have a base plate and edge profile, which are constructed in the same way as containers. The serviceability limits for these parts are the same)

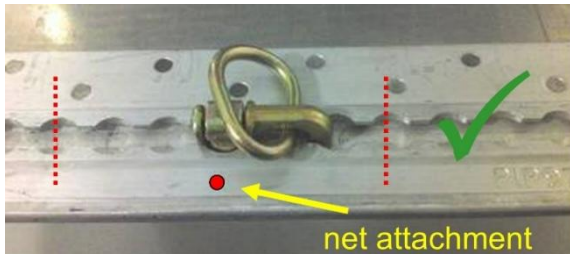


## Edge Profiles of the ULD Base

- ✓ Max. 1 deformation up to 2.5 cm (1 in) per 1 m (3 ft 3 in), or  
Max. 3 deformations together up to 2.5 cm (1 in) per 2 m (6 ft 6 in).



- ✓ Min. 3 undamaged pairs of tie-down track lips on each side of the net attachment point.



- ✓ Max. 2 missing fasteners (rivets) per each edge profile.



- ✓ Fasteners at the corner must not be missing.





## Container / Pallet corners

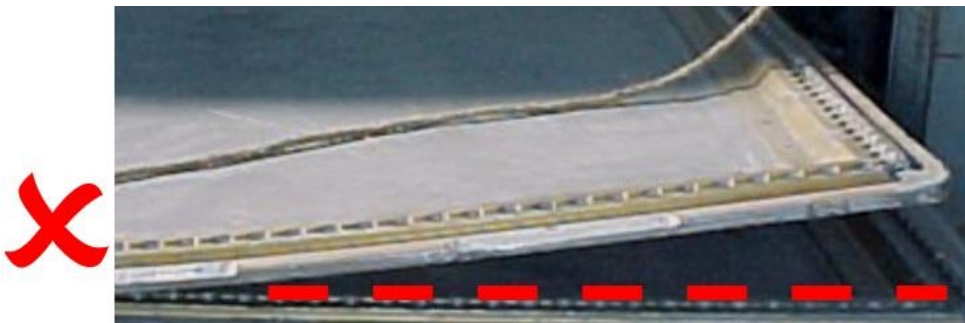
All ULDs with damaged or missing pallet corners are to be considered unserviceable.





**Base Plate:**

- ✓ No deformation larger than 2.5 cm (1 in); this includes the lower outboard panel of an AKE and AKH (following the F- resp H-contour)
- ✓ No deformation that prevents any lock being raised properly over the ULD edge.



- ✓ No holes or cracks in the ULD base. 5 small holes are drilled into pallets, this is intended and for water drainage.





**Panels (walls, roof):**

- ✓ No crack or hole larger than 10 cm (4 in).
- ✓ No more than 2 holes or cracks less than 30 cm (12 in) apart from each other.

Serviceability requirements for wall and roof panels are also applicable for all light weight containers



**Gussets**

- ✓ No gusset is missing, loose, bent or broken.





## Container Extrusion (Frame Profile)

✓ No hole larger than 2 cm (0.8 in) per each frame profile, AND

✓ No crack or deformation longer than 8 cm (3 in) in the longitudinal direction of the frame profiles (extrusions)



✓ No crack or deformation longer than 0.5 cm (0.2 in) in the transverse direction of frame profiles (extrusions)





## Container Roof

✓ No cracks in the upper extrusion (frame)



✓ No deformation of the roof panel greater than 5 cm (2 in)

## Container Base

✓ No crack longer than 8 cm (3 in), AND

✓ No hole greater than 2 cm (0.8 in) in diameter

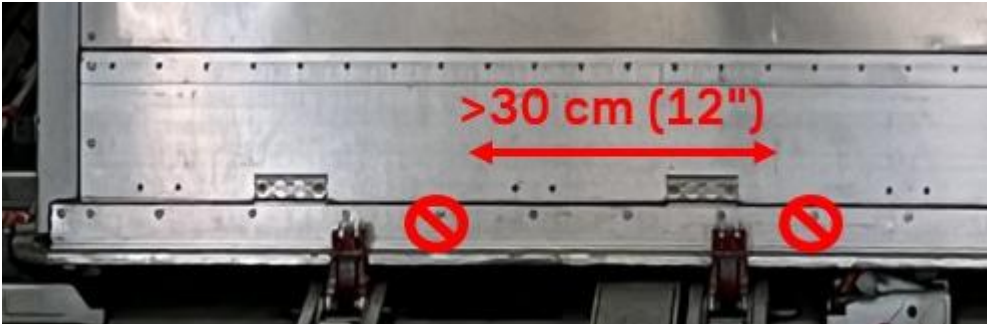




### **Base Fasteners (Containers)**

✓ No more than 2 missing or damaged fasteners (rivets) are found per side.

The distance between the two must be greater than 30 cm / (12 in)



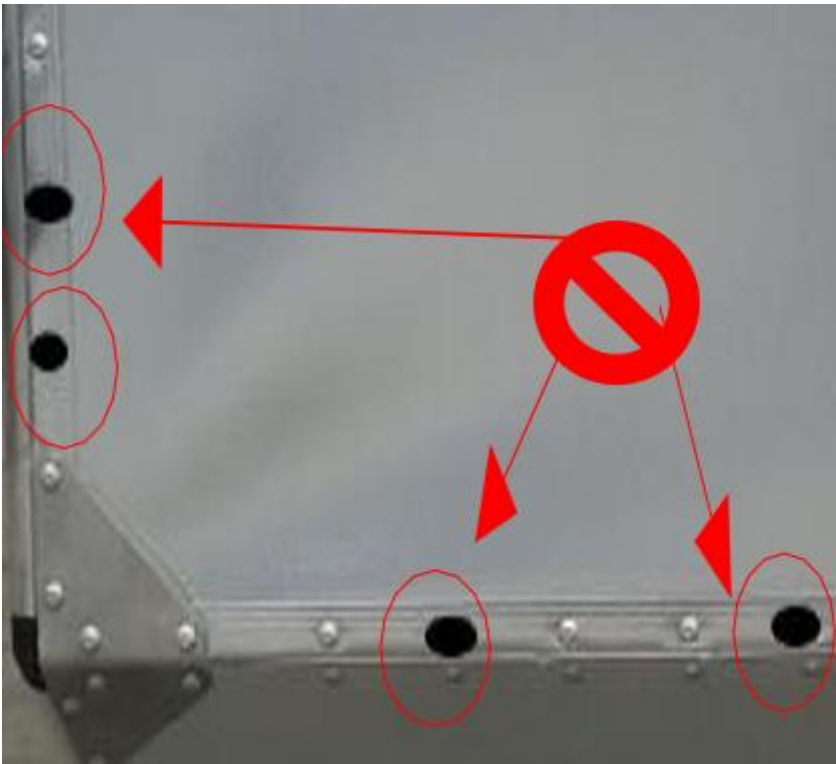
### **Panel Fasteners (Containers)**

Lightweight and Aluminum:

✓ Max 1 fastener (rivet) per extrusion (frame profile) is missing or damaged.

Aluminum only:

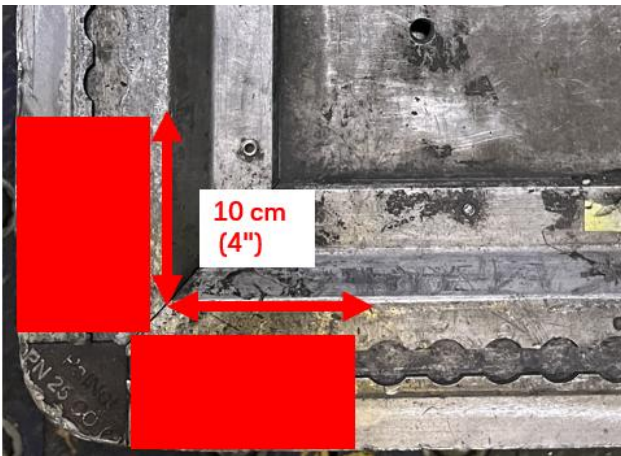
✓ No missing or damaged fastener on the roof





**No Damage Zones:**

✓ No damage within 10 cm (4 in) from the corner of the edge rail



✓ No damage within 10 cm (4 in) of fastener line (rivet line)



✓ No damage within 5 cm (2 in) on curtain edge and locking





**Door Tarpaulins:**

Containers equipped with a door strap net remain serviceable even if the tarpaulin is damaged.

Only for containers where the tarpaulin serves the securing of the load:

✓ Max. two holes or cracks up to 10 cm (4 in) with a min. distance of 50 cm (19.5 in).

✓ All door hooks/tarpaulin eyes must be in place. Deflections of the door hooks are allowed, if the door can still be locked properly at all hooks.

X Velcro or other straps used to lock the doors on containers must not be damaged or worn out.

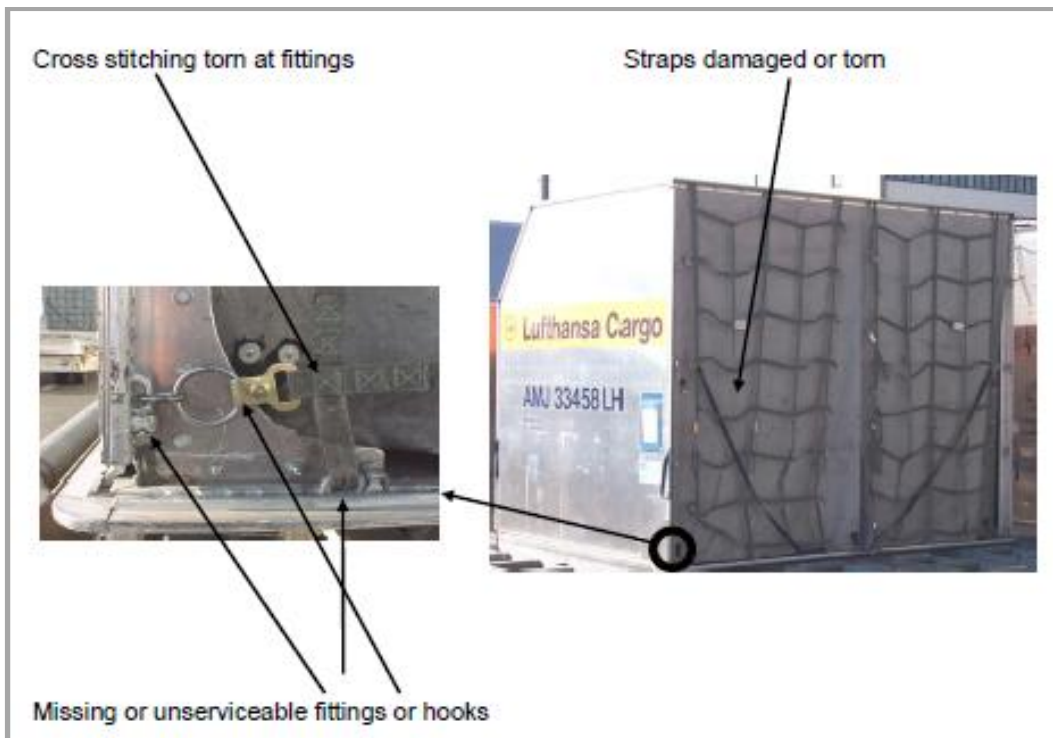
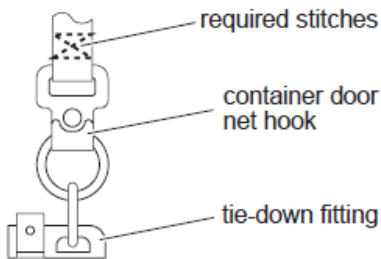




**Door strap nets:**

- ✓ No strap is torn or cut, AND
- ✓ no tie-down fitting or hook is missing, damaged or distorted, AND
- ✓ no stitches at strap connecting fittings are torn or missing.

Note: only the stitches at the connecting fittings as shown in the picture are relevant. Stitches which connect the crossing horizontal and vertical straps are only for stabilization to avoid tangling when folding and unfolding the net.





**ULD Repairs:**

Any repairs must be performed professionally according to the manufacturer's manuals by certified repair shops. Provisional repairs, for example closing of holes by tape, are not allowed.

For ULDs that are unserviceable, do not load cargo onto/into the ULD and return the damage unit to Lufthansa Cargo.

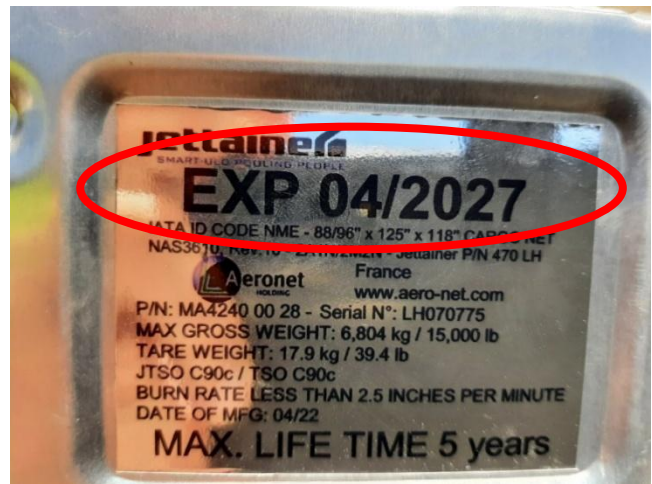
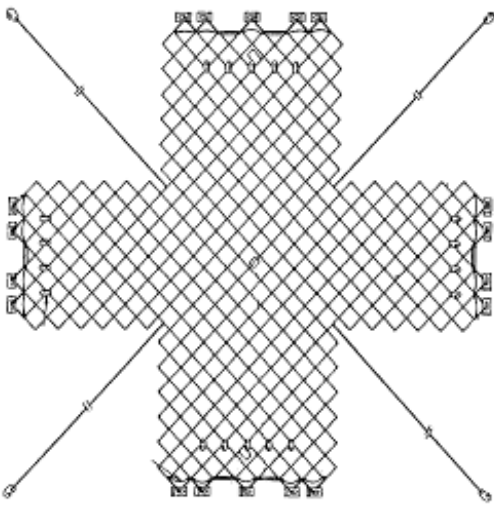




## Pallet Nets

✓ A pallet net is serviceable, if

- the expiry date ("EXP") is not exceeded, which means the net is not older than 5 years, AND
- min **one** type label is fixed and readable, AND
- **all** net fittings and **all** net hooks are attached and undamaged, AND
- **all** corner **lashing lines** are attached and undamaged, AND
- **no** mesh is torn.



If a corner lashing line is damaged or too short (less than 3.7 m, 145 in) it may be replaced by a serviceable red-and-white lashing line from LCAG.





Weight Limitations apply for nets with minor damages:

- 2 minor damages are acceptable.

Please refer to below table for details and the applicable weight limitation.

- **Preferrably use an undamaged net!**

Net Type	used Studs for Net Type	# missing / damaged ...	... Studs / Meshes	... Hooks (*)
NME (PMC/PAJ)	double studs	1	3400 kg	unchanged
		2	2310 kg	5780 kg
NKA (PKC)	single studs	1	790 kg	unchanged
		2	539 kg	1110 kg
NLA (PLA/PLW)	single studs	1	1580 kg	unchanged
		2	1079 kg	2698 kg
NGE (PGE/PZA)	double studs	1	6800 kg	unchanged
		2	4625 kg	12510 kg

(\*) As long as the net can still be secured tightly.

The same above mentioned rules apply for nets of other airlines. Additionally:



- Nets of other airlines must not be older than 3 years starting from the Date of Manufacturing (DATE OF MFG).
- Min. one type label must be fixed for proof of certification, and the label must show markings that it fulfils NAS 3610 or JTSO C90 or TSO C90.
- The weight limit when using OAL nets is 3000 kg. If the cargo is heavier, either use a second net or use a net from LCAG.



## Storage and Transportation of ULD and Nets:

To avoid damages of ULD, the following procedures are in effect and have to be strictly adhered to.

### Storage of empty containers:

- Preferably on roller racks or ball mats
- **On ground:**
  - Only allowed if 2 spacers are used underneath; spacers consist of either plastic foam material or wooden planks/skids
- **Stacking:**
  - On ground up to 2 is allowed (except AMJ/AMH) using spacers underneath and in between
  - In a rack up to 4 allowed using spacers
  - Protection against strong winds is mandatory
  - Container doors (tarpaulin or metal) must be closed to avoid damages of the doors







**Storage of empty pallets:**

- for alignment of pallets, a pallet rack (funnel) is mandatory
- storage of up to maximum 20 pallets piled up on a shelf
- to avoid excessive bending of wing pallets spacers must be used after every 5th pallet.





**Storage of serviceable nets may be done in two ways:**

- Serviceable nets may be kept attached to the pallet on one of the long sides: position the net on the pallet surface and ensure that no part of the net is hanging outside the pallet edge.

**OR**

- Serviceable nets may be packed in a net bag (PN115, one net per sack) and be stored in dry rooms.



- Unserviceable nets must be collected in a separate container (packed in TN0115) and be returned to Lufthansa Cargo.

Never cut off nets that are **permanently** fixed to a pallet! Keep them attached to the pallet as described above also for the return to Lufthansa Cargo.

They are marked with several red signs.





## Storage of loaded containers/pallets:

- must only be stored on slave pallets or roller racks, ball point mats or dollies
- no storage on ground allowed (not even on spacers)!
- never use forklift for moving of loaded containers/pallets!





**Transportation of empty containers:**

- preferably on dollies, slave pallets etc. but allowed by forklift
- on trucks spacers must be used underneath if no roller beds or similar exist
- on trucks stacking up of maximum two allowed (except AMJ/AMH) using spacers underneath and in between





### Transportation of empty pallets:

- a stack of maximum 40 pallets using slave pallets or dollies only



### Transportation of loaded containers/pallets:

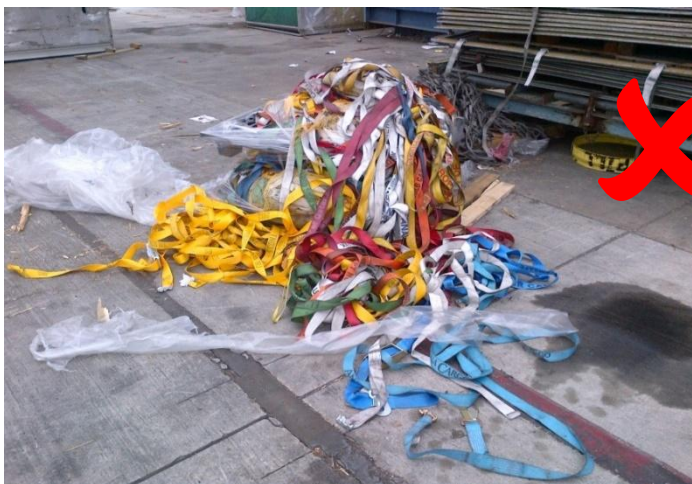
- on dollies allowed only
- never by forklift unless a slave pallet is used
- on trucks/trailers slave pallets, roller beds or ball point mats are mandatory





**Returning empty ULDs and loading material (nets, straps, etc.):**

- Pallets and containers must be returned principally cleaned.
- Straps have to be rolled up for return.
- Nets that are not permanently fixed to a pallet must be packed into net sacks for return. One net per sack is allowed.



**Security Check**

- All ULDs must be checked for hidden objects, which might wrongly be identified as integral parts of a ULD (e.g. double floor).  
By doing this we increase security.



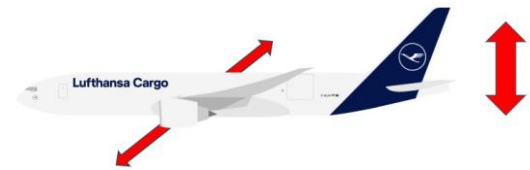


## 6. Lashing

During take-off and climb the aircraft will experience a backward force.



During flight the aircraft may experience turbulence. These are the Up and Down and Lateral forces.



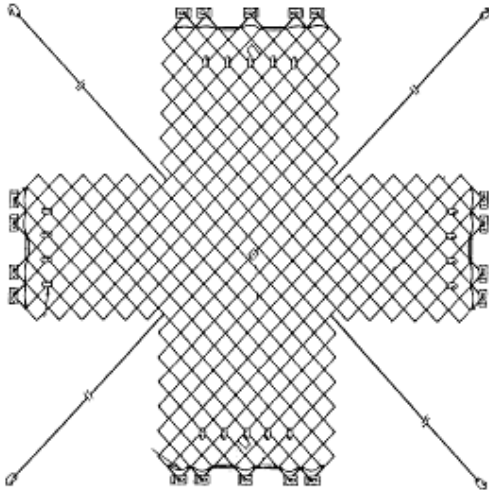
During decent and landing a forward force appears.





If airmail is built up on pallets, a pallet net must be used for lashing.

All pallets with the appropriate net will be provided by Lufthansa Cargo.



Preferably Lufthansa Cargo nets are to be used on Lufthansa Cargo pallets. If you use a pallet net from other airlines, please refer to the regulations in chapter 5, especially the weight limitation up to 3000 kg.



On the pallet edge net attachment points are located. These markings tell where the net has to be fixed to the pallet, 5 fittings on the long side and 4 fittings on the shorter side of the LCAG 88/96" pallets. All net fittings must be utilised.

All 4 corner lashing lines have to be available (not missing or damaged).

It is not allowed to increase the length or repair the nets with ropes or straps.



## 7. Loading Principles

- Before using a ULD the serviceability **MUST** be examined.
- ULDs must be handled with utmost care.
- ULDs must only be loaded onto ULD Transport dollies or other appropriate roller bed equipment.
- Heavy and sturdy pieces are to be loaded as a first or bottom layer. They should be stowed towards **the centre of the pallet**.
- Use bricklayer method to stabilise Airmail, whenever possible.



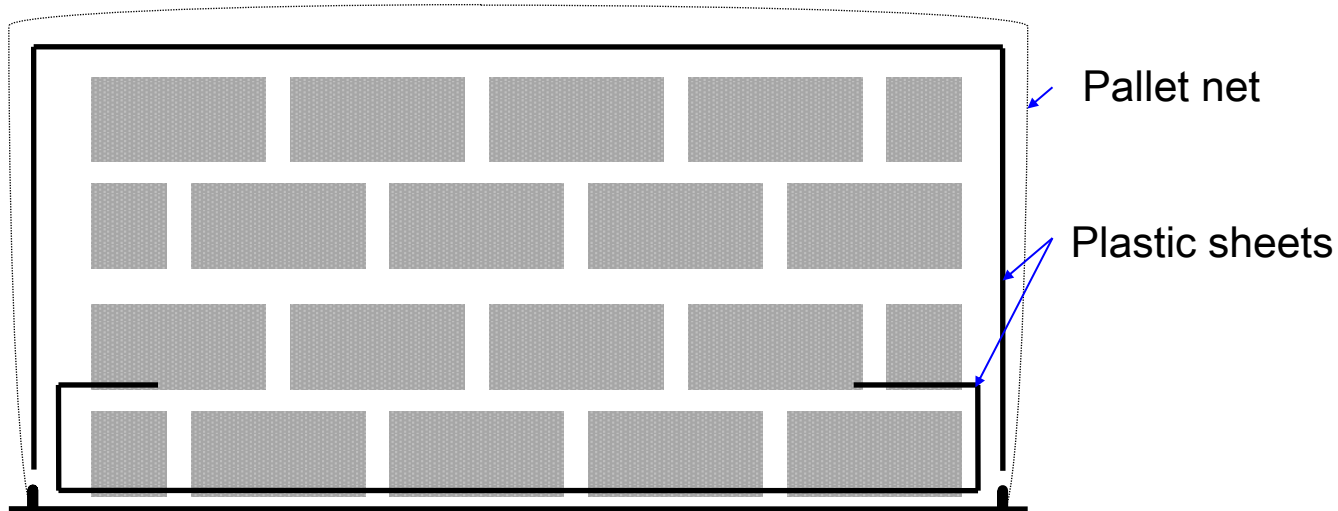
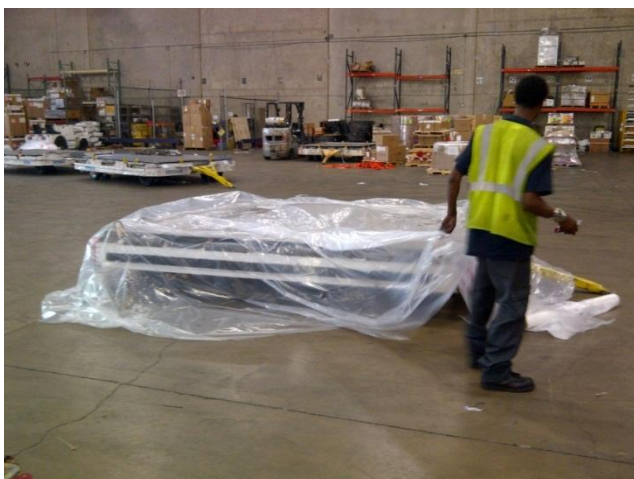


## Securing of Airmail loaded on a pallet

Cover the base of the pallet with plastic.

Airmail is to be protected against rain and snow by using plastic foil or sheeting. The plastic sheeting underneath must be tucked in between the first and second layer of Airmail.

The plastic sheeting over the top must be pulled down so that it covers the plastic protecting the bottom layers. To keep the plastic in place and to avoid it hanging over the edge of the pallet, we suggest using adhesive tape. Afterwards make sure that there are no tears or holes in the plastic, otherwise start over again.





## 8. ULD Tag

The Lufthansa ULD Tag is mandatory for all ULDs. Alternatively the IATA ULD tag may be used.

Responsibility of the BUP Postal Authority:

- must be BUP qualified
- necessary fields on the ULD TAG must be filled out
- to confirm that the ULD has been correctly built-up you must enter your B-Number, and if not available, the signature / name, as the agent takes full responsibility for the build-up (Point 4)

- 1 ID-Code ( ULD Number )
- 2 Destination ( Airport of Destination )
- 3 Mark the BUP box with "X"
- 4 B-Number otherwise Signature/Name of Agent (Signature for correct build-up according to the LCAG guidelines / requirements)
- 5 Loaded at ( Airport of Loading )
- 6 Flight No. ( Flight number and date )
- 7 Contents „M“ ( = Airmail )
- 8 Remarks for special loads = Special Load Codes and BUP License type



Lufthansa Unit Load Device		
ID Code: AKE 12345 LH		
Destination	STD	
MIA		
Net Weight (kg)	BUP <input checked="" type="checkbox"/>	
Tare Weight (kg)	Signature of Agent	
<b>Total (kg)</b>		
Loaded at	Flight No.	Position on a/c
FRA	LH 462/18	
Transfer at	Flight No.	Position on a/c
Contents		
M		
Remarks for special loads		
BUPairmail		

Form 093000 Z-18 (FRA F/HG)



IATA ULD Tag for BUP units

If a BUP PA uses the IATA ULD Tag, the PA must also add their B-Number otherwise signature/name and license in the "Remarks" field.

 <b>CONTAINER/PALLET</b>		
<b>AIRLINE</b>		ID CODE .....
<b>DESTINATION</b>		
<b>NET WEIGHT (Kg)</b>		
<b>TARE WEIGHT (Kg)</b>		
<b>TOTAL (Kg)</b>		
<b>LOADED AT</b>	<b>FLIGHT</b>	<b>POSITION ON A/C</b>
<b>TRANSFER AT</b>	<b>FLIGHT</b>	<b>POSITION ON A/C</b>
<b>CONTENTS</b>		
<b>REMARKS:</b>  <div style="text-align: right;"> <p><b>BUPairmail</b> <b>B-Number or</b></p>  </div>		



# *Happy Loading!*





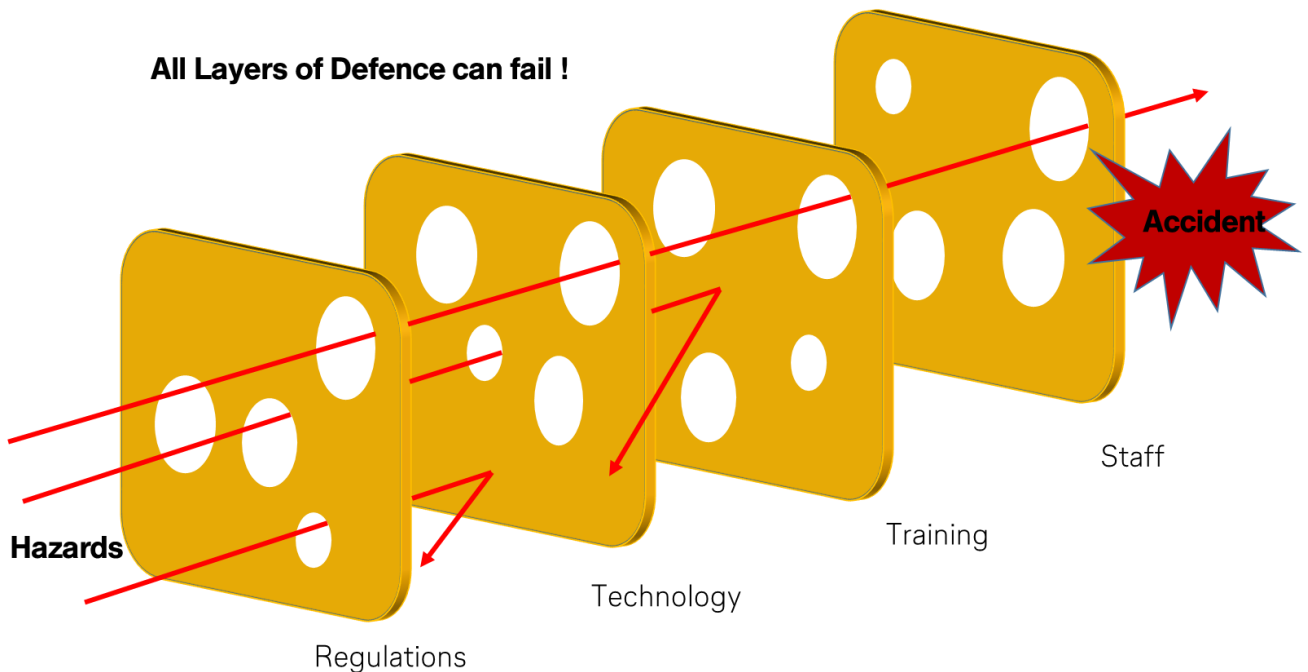
## Appendix

### THE HUMAN FACTOR

#### Humans are making mistakes

Human error is the unintentional act of performing a task incorrectly like:

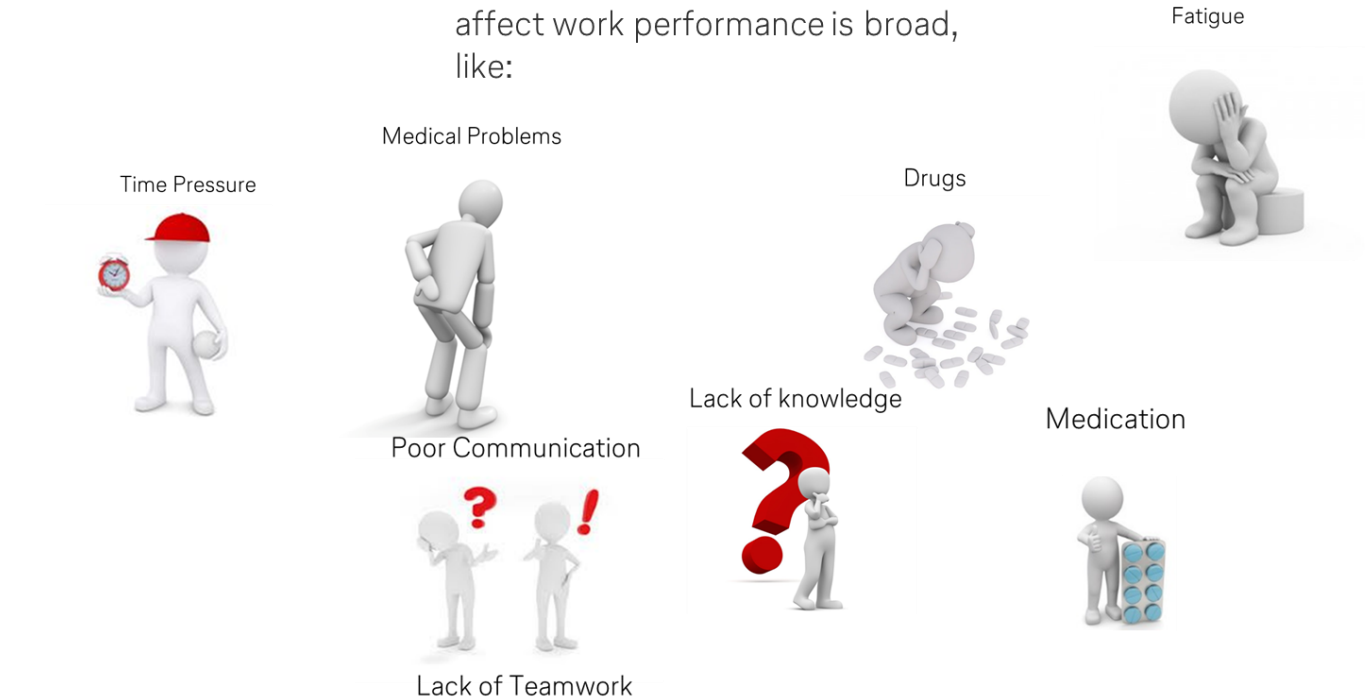
- Not performing a task or an act
- Accomplishing a task incorrectly
- Performing a task not authorized





Human error, rather than mechanical failure, underlies most aviation accidents.

The list of human factors that can affect work performance is broad, like:



In Aviation every Hazard or Risk can lead to an Accident.

You can reduce those risks.

Keep yourself fit

Get enough rest

Don` t work if you don` t feel okay

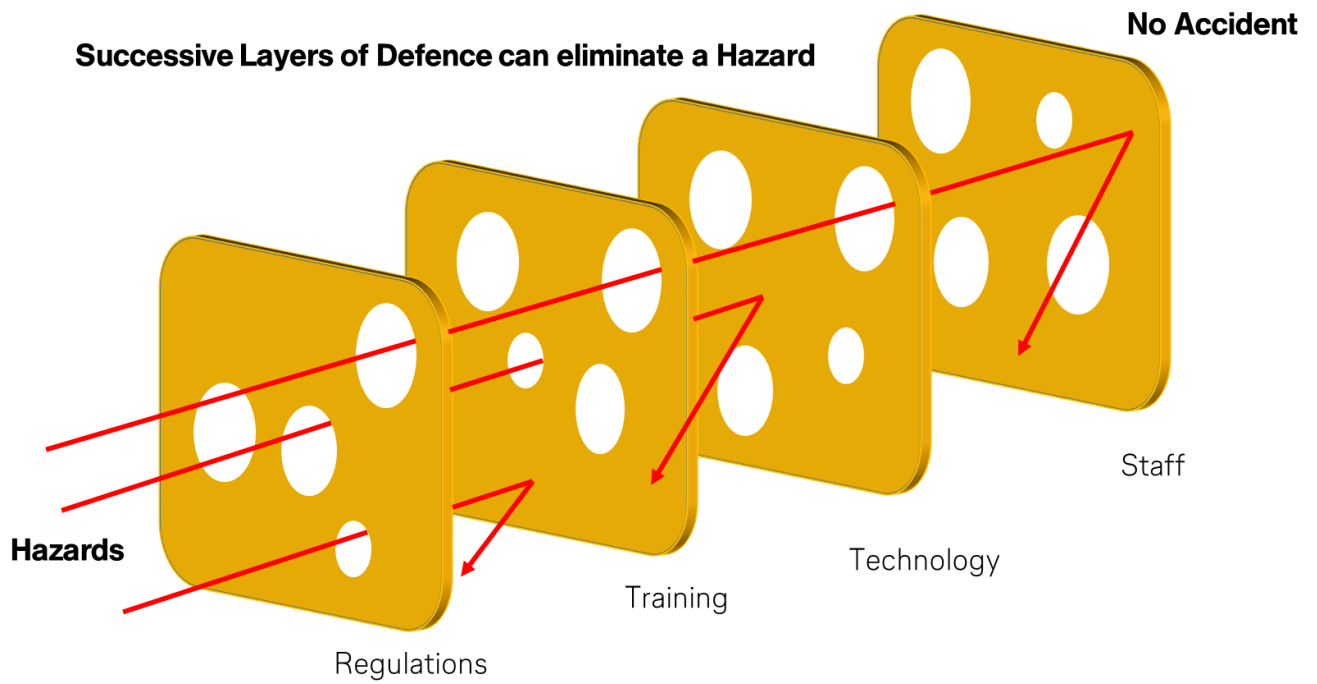
Keep yourself up to date

Always expect to find something wrong

Never sign off on something that you did not fully check

Always double check your work





And if you have identified a risk or made a mistake ....

**REPORT IT !!**

Even if it is against human nature to show weakness, report what happened,  
And support others by what has happened to you.

**MISSION ACCOMPLISHED!!**

